

The Local

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Scratchbuilding a Small Wooden Freight Station and Platform

By Martin Brechbiel

SO, NOW THAT YOU HAVE A FEW old time wooden freight cars built, you need a place to stop and load and unload those items you've carefully crafted as flatcar loads or packaged goods from your gondolas.

There are countless examples in the model railroading literature that you can use for plans to create a small freight station or platform (try www.index.mrmag.com). With a few good pictures, an article or two, or just a photocopied page from a borrowed book, you can be on your way to the workbench! Or, more likely since you need to fit this into a specific location or limited space on your layout, you can just build to fit the space and location on your layout.

I continue to advocate for building in wood since wood really looks like...wood, cuts like...wood, stains like...wood, and paints like...(you guessed it)...wood. There are several suppliers of scale lumber and various types of specialty scribed sheet lumber: Northeastern, Kappler, Mt. Albert, and others. You're going to need the standard tools, e.g., a fine kerf razor saw, miter box, small square, sandpaper, and a few very sharp modeling knives. Invest in a good scale ruler and a fine marker; 0.5 mm mechanical pencils are nice and you can get colored leads for them. A pin vise and a set of drills, tweezers, files, and gluing weights are required as well.

Assembly requires glue, and since there are butt joints, my adhesive of choice remains Walther's Goo or some equivalent. A generic brand of medium viscosity CA adhesive works just fine for nearly every application.

So let's get building! We'll build the platform and station separately, and then combine the two to finish. One of the advantages of working this way is that while the glue is setting on the platform, you can work on the station.

THE PLATFORM

Just like a flatcar, construction begins with building a perimeter box frame. Working from Figure 1 (see page 4), assemble the framework as square as possible using a dab of Goo on the ends of each joist to tack them in place against the end sills. These are all made from 1/4" x 3/16" lumber. You'll need four 29'-9" long and two 20'-0" joists and three end boards measuring 14 ft, 9 ft, and 5 ft respectively. Of course, these dimensions are flexible; you can lengthen, shorten, or widen this platform to suit your needs in location and function. And, of course, since this is laid out in O-Scale, everything is easily translated to your favorite scale.



Let's get some deck boards down. Scribed sheeting makes this fast and easy, but I like individual boards. Odds are no one will look under your structure, but having some random widths and having an aged, worn surface adds character. I like to use those wooden coffee stirrers that you can buy by the case of 10,000 or you can buy the expensive commercial model-building version, Doctor Ben's Rustic Lumber. I also like to pre-stain these using a complex mixture of Minwax. The complexity is that I have no idea what's in the bottle; I just

continued on page 4

Keeping In Touch...

By Fred Miller, MMR
MER Business Manager

AS WE MOVE towards the single NMRA dues structure to be effective this fall and the MER's plan to charge only for subscriptions to **The Local**, questions have been raised about the members who have paid MER dues for periods beyond the 9/1/05 date and MER Life members who have paid for life membership. Perhaps I can answer those questions.

The MER Life members are assured of receiving **The Local** for their lifetime. This can take the form of a printed copy of **The Local** sent through the mail as in the past, a copy distributed electronically, or both. The choice is theirs.

The Regular members with MER dues invested beyond 9/1/05 will have their dues (pre)payments converted into subscriptions to the printed version of **The Local**. Each dollar of prepaid dues beyond 9/1/05 will "buy" one issue of **The Local**. (Remember dues have been paid at \$8/yr or 67¢/mo. and **The Local** is \$6/yr or \$1/issue)

Those members who have renewed recently have seen their membership cards reflect both an MER dues expiration date and an expiration date for their subscription to **The Local**. Beginning with the Sep-Oct issue of **The Local**, all Regular members will see only their subscription expiration, not dues expiration, displayed on the mailing labels.

Of course Regular members can also elect to receive **The Local** electronically in addition to, or instead of a printed copy. A good many members have already converted to receiving it electronically only. If you have an e-mail account, why not give it a try. We can set you up to receive the electronic version on a trial basis in addition to your paper copy. Remember, there is no charge for receiving the electronic version of **The Local**. 

The Local

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The Local welcomes articles, photographs and model railroad related material as contributions to members' mutual enjoyment of the hobby. Materials should have a wide appeal. Editors will exercise all due care of submissions, but contributors should not send originals without having back-up copies of both articles and photographs. Editors, by definition, reserve the right—and have the responsibility—to make corrections, deletions and changes to accommodate space.

MER web page URL is <http://members.cox.net/chyde/MER/MER.html>

President's Column

By Clint Hyde

IN FEBRUARY, VP Noll Horan and I went to a Susquehanna Division meeting at Bob Martin's house where we got to do the usual things: meet new folks, say hi to others we hadn't seen in a while, run a train for a bit (picking up a few AP Dispatcher points), look at some nice models, watch a modeling clinic about paints, and yak a good bit. Always a good time doing all that. Did much the same a few weeks later at a South Mountain Division meeting a little closer to home. Keep us informed about when and we'll try to come to yours! Worth doing, visiting those other divisions, I'd recommend taking the opportunity if and when you have it.

The fall convention is shaping up pretty well. There will be some interesting items, so I won't steal their thunder by giving away anything too exciting. Something I've been trying to push into existence for the past few years is finally close enough to have a first pass this year. You've undoubtedly read about "Modeling with the Masters" in the NMRA **Scale Rails** magazine. If you've been to recent MER conventions, or recent-to-slightly-older conventions in the Potomac Division area, you'll have seen or attended the hands-on make-it-and-take-it clinics. I've been pushing for these for several years now, with the intent that we ultimately have MER's own "Modeling with the MER Masters" series. MER has plenty of very fine modelers, skilled folks whose abilities are visible either near where you live, or models shown at the convention (and photographs here in **The Local**).

Our first multi-clinic sequence should take place this fall, featuring Noll Horan on weathering wood, myself on airbrushing things like doors and windows, and Martin Brechbiel on scratchbuilding with wood. See Martin's story on page one for a picture and writeup of this very item. The intent is that you sign up for the trio, get the bagged structure kit, and begin by weathering the wood, then airbrushing other pieces like the doors and windows (clinic will cover more than that), and then assemble the thing with Martin, possibly followed by a return to airbrush some weathering on the completed structure. We intend to have the kit available in HO and O; we're doing a trial run of it (O only) at the Potomac Division mini-convention at the end of April to work out problems with timing, etc. Attendance will be limited to 10 O and 10 HO kits. You're welcome to look in on the assembly part; the weathering and airbrushing will be general clinics, both hands-on, but emphasizing the kit parts. I hope to do the airbrushing outside, with good weather.

Keep your eye out for future offerings along this line; and, I'm open to suggestions about other variations on this theme.

Electronic registration is now available for the Raleigh convention, and our new Business Manager, Fred Miller, now handles the **eLocal**—the electronic version of **The Local**. Let him know if you're interested in receiving it.

You should be receiving a small ballot shortly, if you haven't

already, regarding the new MER bylaws. Please get your vote in quickly. We already have a topic to discuss as an amendment, we'll get on that later this year.

I saw John Roberts, NMRA President, at GSMTS Timonium in April. He informed me that he'll be moving to Williamsburg around the end of 2005, which means we'll be seeing more of him soon, at JRD and Tidewater events, and MER conventions, etc.

Model Railroading is fun! Don't forget that. Quiz on Friday. 

Key West's Over-Sea Railway Historeum

By Nicholas Kalis

IF YOUR TRAVELS should take you to the Florida Keys, don't miss Flagler Station's Over-Sea Railway Historeum. During spring break, I had the pleasure of visiting this Key West museum and enjoying the three videotapes played on-site that tell the story of "The Eighth Wonder of the World". Presented by Historic Tours of America, this museum tells the story of Henry Morrison Flagler and his quest to reach Key West by rail.

In 1905, one of the world's richest men, Henry Flagler, announced his plan to link Miami with Key West by rail. Flagler had been a partner of John D. Rockefeller in Standard Oil. Eight years, 30 million dollars, and hundreds of lost lives later, Flagler disproved the skeptics and accomplished his mission. In 1935 Flagler's prize was destroyed by a hurricane. Visitors to the Keys can still see many of the steel trestle bridges and concrete viaducts that remain of Flagler's Key West Extension. In addition to the museum and its gift shop, one will find the historic Maggie Atwell House and a restored FEC paymaster's boxcar. I particularly enjoyed what appeared to be an O-scale model of how the railroad bridges were built.

Flagler Station is Stop #2 on the Old Town Trolley and the only stop on the Conch Tour Train. Flagler Station is located at the Historic Key West Seaport on the corner of Margaret and Caroline Streets. Admission is \$5.00. For more information about the Railroad That Went to Sea contact the Historeum at 901 Caroline Street, Key West, FL 33040, (305)-295-3562, or visit their web site at www.flaglerstation.net. 

randomly top off a small bottle from whatever can is handy on the shelf and variably dilute to suit.

Cut enough boards (about 40) to cover the platform. Make them a bit longer than the width of the platform. You can set them flush on one side and prune off the excess overhang later.

are willing to match up complementary boards so things work out evenly. Trim the deck boards flush with the side sills. I use a band saw or a construction knife to trim off the overhand along with a belt sander, but a razor saw, albeit slower, also works fine.

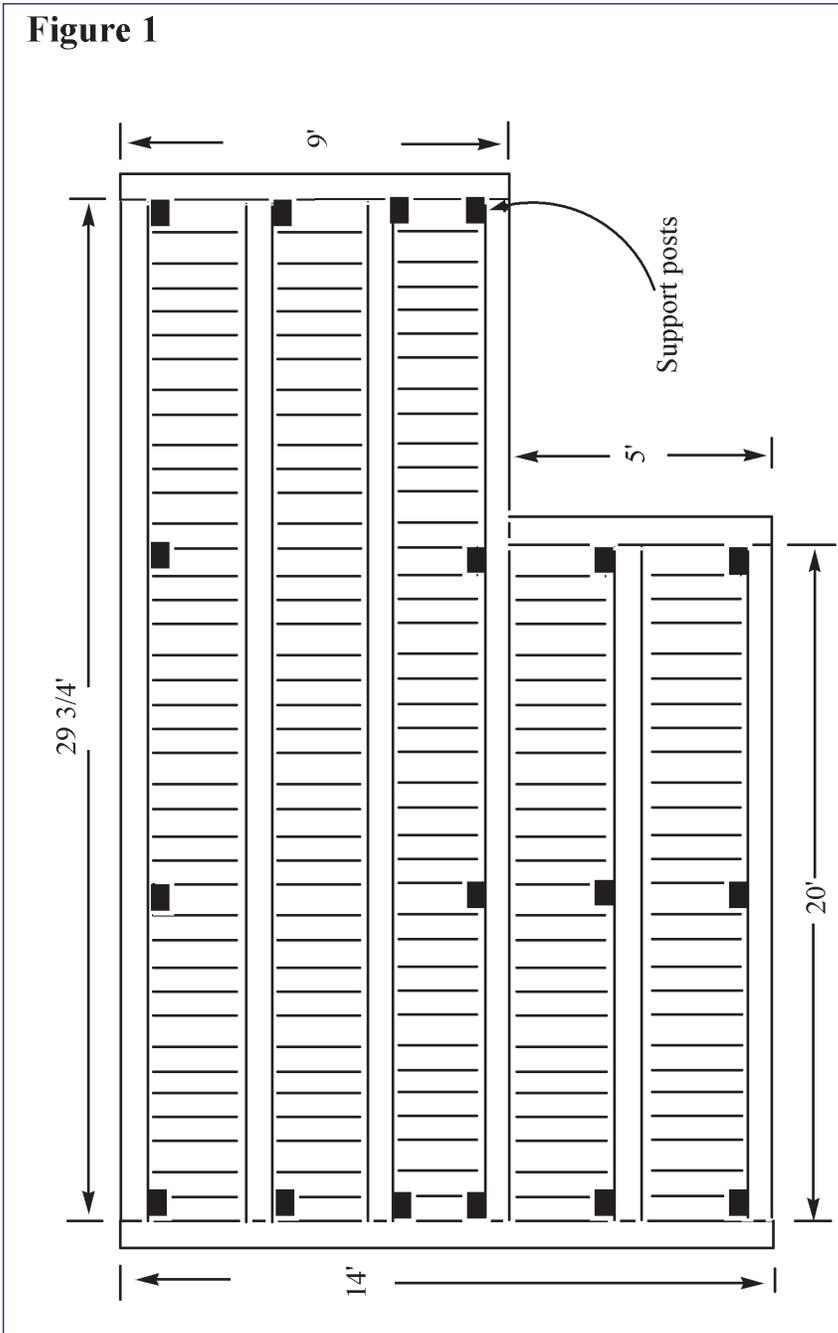
The platform needs some legs to stand on. I used HO-scale 8 by 12s (that I had on hand) cut 4'-6" (O-scale) long to bring the height of the platform up close to the opening on one of my boxcars. A square or other dimensional lumber leg would work just fine also. The locations for these are marked out in Figure 1. Just glue them straight into place against the joists and sills. After you get this done, add diagonal bracing (see Figure 4 on page 6). I used HO-scale 3 by 10s for this, but again, other dimension lumber that looks right will be fine.

Now let's add a ramp. Take two more pieces of that 1/4" x 3/16" lumber, cut them to the full length of the ramp, 16 scale feet in this case, and create the angles at each end by sanding and test fitting until satisfied. Glue a piece of scale 4x4 across the bottom of the platform end sill for the ramp joists to rest on. Notch the ends of the ramp beams to accept this 4x4, glue them in place, and cover them with short pieces of decking boards. This will be a little delicate to trim neatly later, so it's worth the time to cut these boards close to correct length now. I added nut-bolt-washer castings (Grandt Line #23) to dress up the assembly where the legs are bolted in to complete the platform assembly. You can use this platform as is or you can add the station.

THE STATION

Figure 2 (see page 5) shows the four walls of the station. The sides and ends are cut from Northeastern 3/32" clapboard siding. One side and end are taller than the others because they extend over the side of the platform. I have a window in one end with a door at the opposite end. These could be moved to suit and different or more windows could be used. It's up to you to take my plan and customize it to suit your needs. Before you cut out the holes for the door and window, cut four, 9-scale-foot lengths of O-scale 4x4 and glue one to each inside edge of each end measuring from the top down. These serve two purposes. First, they stabilize the clapboard siding from splitting when you make cut outs for the door and window. Secondly, they provide a surface for gluing the sides to the ends when you assemble the structure. See Figure 3 (see page 6). Run another length of O-scale 4x4 along the base of the shorter walls to provide an attachment site to the platform. Having done this step, decide on your window and door locations. Mark their openings using the Grandt Line castings themselves as guides. The openings in the figure were derived from a Grandt Line #3707 window and a modified Grandt Line #3612 door; the styles and locations could be easily changed to complement your personal tastes and functions. Using a very sharp knife, repeatedly scribe a line until you cut through the siding. Do not rush this! Even with the 4x4 reinforcing, the clapboard siding can crack or split. It's repairable with CA, but avoiding this situation is preferable. Test fit the castings in their

Figure 1



Pick out four to six nice straight deck boards and, after laying down a bead of CA on the top of the end sill and down the joists for a distance of about three deck boards, glue them down. This will "lock" your joists and frame squarely in place and let you finish installing the rest of the deck boards. Try to pick "interesting" boards; arrange for color patterns, grain, "knots", etc., to be visible. A little damage adds character, but also stay with straight boards. Avoid using seriously crooked or warped ones unless you

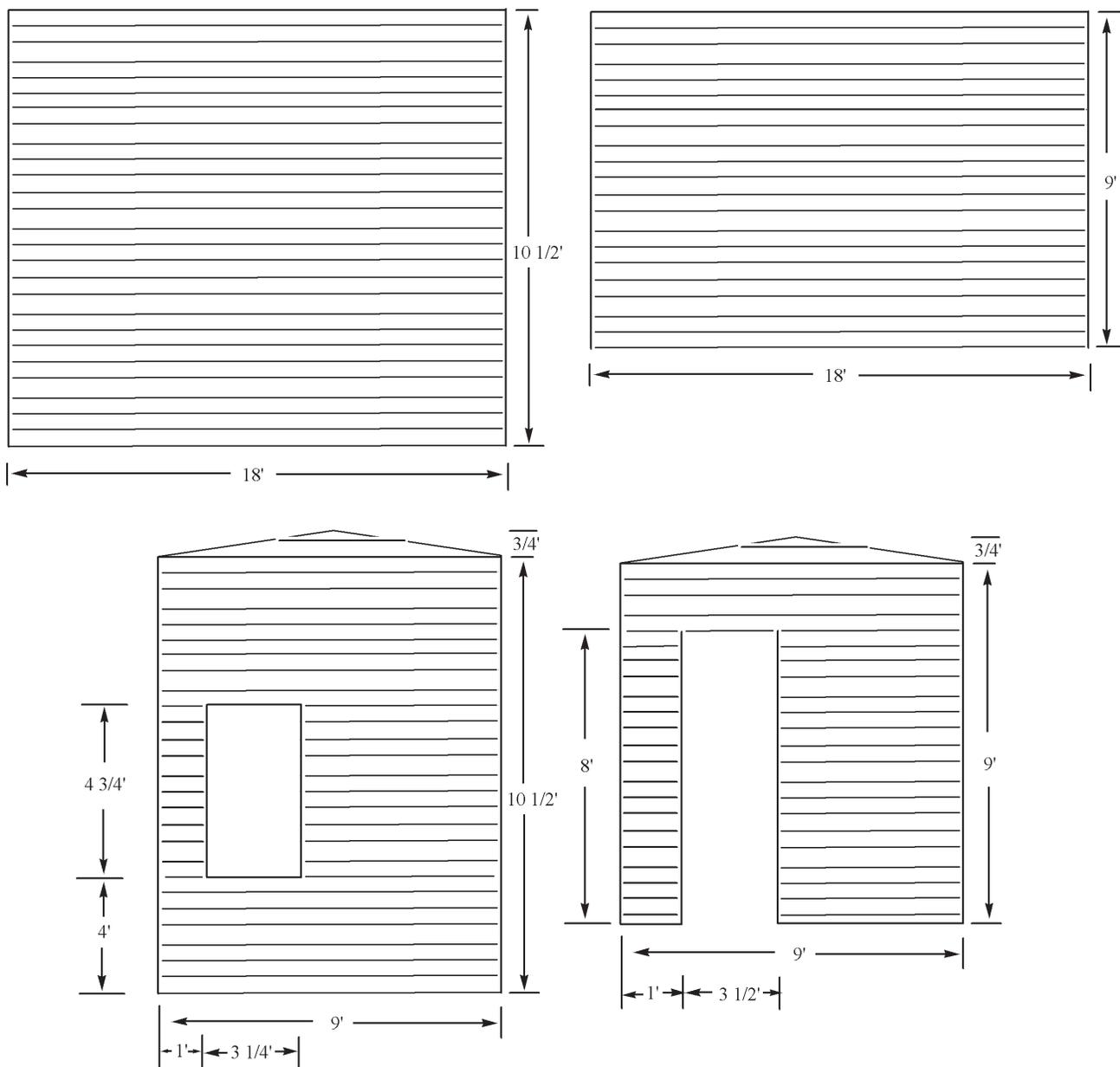
corresponding openings and make “adjustments” with your knife as needed so they fit snug but not tight. This gives you the opportunity to paint the door and window castings and glaze the openings with slip cover glass prior to installation.

Now, assemble the four walls. Carefully run a bead of glue on one of the O-scale 4x4 edges on the interior of the “door end” and glue this to the “short wall” making sure to align the tops of the two pieces. Repeat this step gluing the “window end” to the other end of the short wall. Try to get these joints as close as possible to 90 degrees. Now, glue the “tall wall” to the two ends. Start with one end; get it carefully aligned at the top, and square, and then

let the glue set up well. You can then carefully pry and glue the other end of this wall to the remaining end wall.

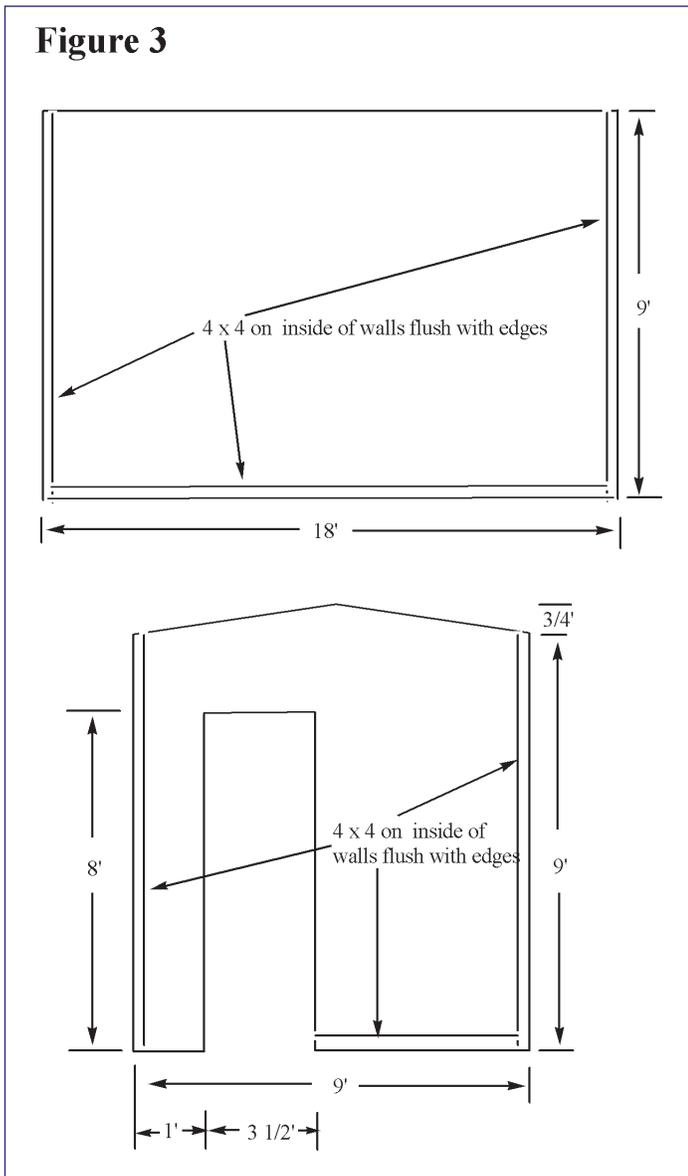
Now, let’s get a roof on this structure. You could do board over rafters and then put on shingles, a metal roof, or a quick simple tarpaper roof. We’ll do the latter, but with a simplified roof under it. Using two pieces of 1/32” thick sheet basswood sheet (7 by 20 scale feet), close up the roof leaving a one-scale-foot overhang on the sides and a six-inch-scale overhang on the ends. Flip the station over and glue in false rafters made of 1/16x5/32 strip wood sanded to the correct bevel to sit snugly against the top clapboard, meeting the roof overhang and extend-

Figure 2



continued on page 6

Figure 3



ing out to the near the edge of the roof. Start at each end and set these in evenly at about a scale two-foot centering. My model has nine rafters. Make sure your rafters line up on both sides! For tarpaper you can use sheets of tissue glued down, but I like to use 600 grit sandpaper cut into 4' x 8' sheets. Overlap these slightly,

starting from the lower edge corner and work your way across. Two more rows should do it; make sure to stagger the seams. Sheets folded in half cap the roof. These can be glued down with a minimal amount of thinned wood glue or CA. You can cut a hole in the roof or surface mount a smoke jack at this point. Grandt Line makes these although mine is an ancient Scale Craft casting that I drilled out and Goo'd in place. Two places that you need to do a little trim work are on the station corners and end fascia. Milled angle wood is a little too thick for this so I built the corner molding to cover the joints of the clapboard with HO 2x8s. This same scale lumber works well for the fascia boards, too.

Now it's time to paint the door and window castings, and glaze them with some glass slide slipcovers. I hand painted mine with two coats of Polly S. For those of you who like convenience, Stevens Creek (<http://www.hon3.com/glazing.htm>) sells glazing pre-cut for some of the Grandt Line window and door castings. I still like glass because plastic doesn't quite look right. This very thin glass can be accurately cut with a diamond tip scribe sold through scientific supply houses. Try Thomas Scientific or Fisher Scientific. Minimal pressure and patience are the key ingredients for success here. Secure the glass to the interior of the painted door and window with minimal ACC or Goo.

PUTTING IT ALL TOGETHER

Having completed these steps, it's a simple matter to mount the building on the platform. A bead of CA on the scale 4x4s you added at the base of the walls and to the part of the walls that overlap the platform sides secures the building to the platform.

FINISHING IT UP

I first stained the entire building with Minwax Pecan. Then I took some very old Floquil Dark Green, diluted it with mineral spirits and with the cheapest brush available spread it around until I got the coloration and depth that I wanted; darker under the eaves and protected spots and lighter away everywhere else. I followed this with some streaking of the window paint below the sill. The tarpaper roof and the platform lags and braces were painted Polly S Oily Black, and the smoke jack got a coat of Polly S Steam Black with a few touches of Roof Brown. Next I added some details – crates from Berkshire Valley, some leftover passenger car seats by Grandt Line, and a Berkshire Valley box with a hinged top that I liked. I also added a hanging sign made from scrap wood. I gave it the same treatment with the green paint and lettered it with Clover House dry transfers. To give the sign

Figure 4

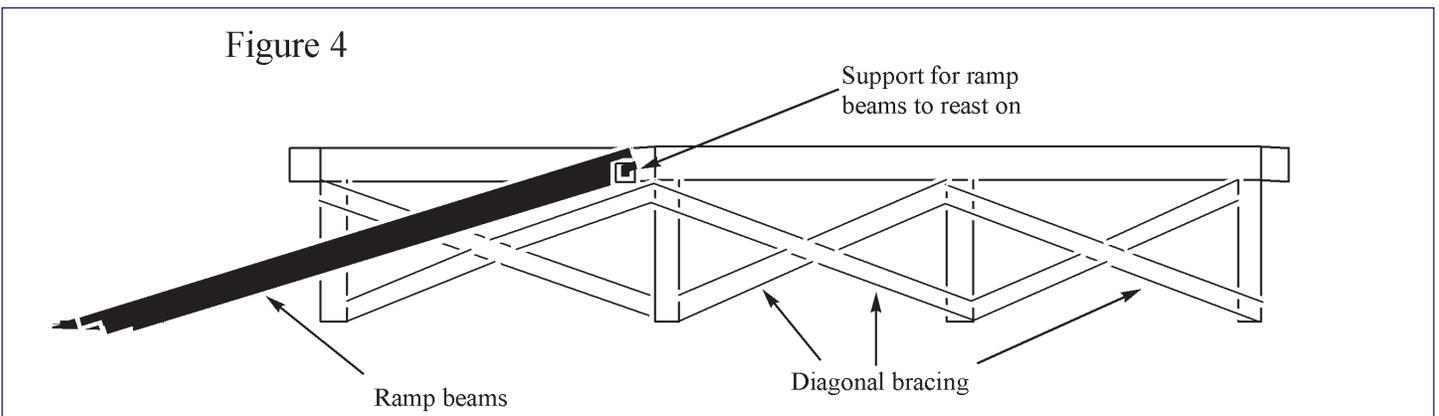
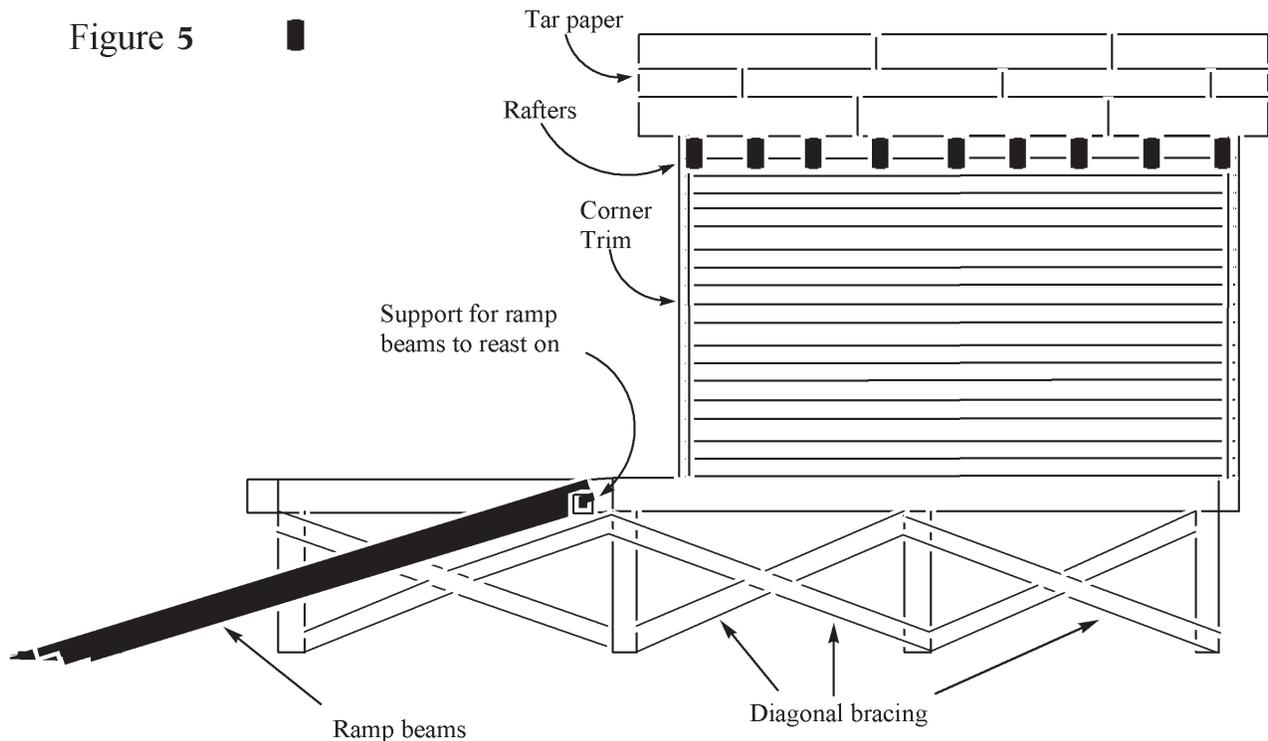


Figure 5



a weather-beaten look, I sanded off some of the lettering.

Finally, I wanted to add something to the blank wall. I had a jpeg file of an old tin sign advertising “Mavis” chocolate drink. I cropped the excess from the picture using MS Photo Editor, imported it into ChemDraw (non-chemists can use PowerPoint or a host of other software for manipulating jpegs), and sized it to fit. After printing it on an ink jet printer and cutting it out, I carefully sanded the paper very thin. Since the colors were far too bright and I also wanted the paper to be more pliable to fit the contours of the clapboard, I soaked it in water for about 30 minutes. This resulted in washing out much of the coloring and providing very fragile paper that I laid down over thinned carpenter’s glue on the wall. I used a block of scrap wood to work



the paper into the clapboards and let it dry. When the glue dried, I sprayed on some Testors Dullcote. To further tone down the colors, I gave the entire structure a light airbrushing with grimy black and roof brown.

You could, of course, add a complete interior with lighting and a removable roof, but the primary objective here is to build a simple, first structure that becomes the basis for future and more complex endeavors.

Keep on building! 🛠️





Independence Junction Update

THE PHILADELPHIA DIVISION'S planning for the 2006 National Convention in Philadelphia (July 2 through July 9) is proceeding. Currently the Committee is hard at work planning and coordinating the prototype tours, layout tours, and fan trips. The Prototype Tour team is targeting several tour sites. These are scheduled to include ISG (former Bethlehem – Lukens Steel Mill), Carpenter Steel, PATCO river line & shops, Amtrak (CETEC Northeast Control Center, Bear & Wilmington shops), SEPTA (Frazier, Overbrook & Wayne Junctions Shops, Reading Tech. Society (including the Reading & Northern RR), and the Lehigh Cement Plant.

Our fan trips will include the Strasburg Railroad and the Pennsylvania State Railroad Museum in nearby Lancaster County. Lancaster County is home to the world famous Amish country. There will be more than 30 local layouts open along with Operations Callboard, which allows modelers to sign up to operate a variety of layouts.

Philadelphia, an Exciting Place to Visit

The convention will highlight the prototype railroading around the Philadelphia area. Philadelphia was most recently home to Conrail and is still central to the Conrail Shared Assets Operation. Both Norfolk Southern (East – West routes) and CSX (North – South routes) serve the city. Both railroads also operate major classification yards in the region.

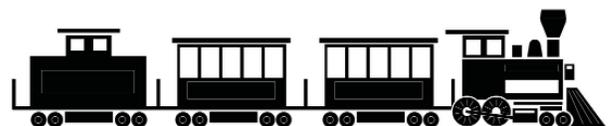
Passenger rail service is alive and well in the Philadelphia Region. The nation's fourth largest city is home to SEPTA, the nation's third largest

passenger rail system. SEPTA operates a variety of light rail and heavy rail lines, including eight commuter lines all radiating out of Center City. Five traditional trolley routes and two suburban trolley routes along with the subway and elevated system highlight the variety of the area. Amtrak also calls the region home to the Northeast Corridor operations. The CETC Northeast Corridor Control Center is one of the prototype tours included during the week of Independence Junction.

The convention also takes place at the same time as Philadelphia's Welcome America festivities for the Fourth of July holiday. There will be hundreds of free events including fireworks and an old fashioned parade that will pass right in front of the convention hotel. Other local attractions include the Philadelphia Museum of Art, the Franklin Institute Science Museum, the Philadelphia Zoo, the Camden Aquarium, and the Battleship New Jersey.

With all the rail and non-rail related activities Independence Junction will be fun for the whole family! Make your plans now to attend! You won't want to miss it!

Registration information should be sent to:
Independence Junction 2006
514 Dover Place
St. Louis, MO 63111-2338
www.ij2006.org



Special Award Sponsored by the MER to be Inaugurated at the Cincinnati National Convention

*By Ray Bilodeau,
MER General Contest Chairman*

AT THIS YEAR'S NATIONAL CONVENTION in Cincinnati, the Mid-Eastern Region will inaugurate and sponsor a model contest special award. The award will be called the **James L. Teese Memorial New Modeler Award**. This MER sponsored and funded prize will be given to a modeler, a first time entrant, who puts a model in and then earns the highest score among first-time modelers entered in the judged national model contest held annually at the NMRA national convention.

This award recognizes the contributions to the MER and NMRA of James L. Teese, MMR, a long-time member and fine model builder from the Charlotte area and a member of the Carolina Southern Division. Mr. Teese was an extraordinarily creative model builder. He worked in both O and HO scales and earned the title MMR #190 in 1992. He passed away in October 2004. Jim was always willing to lend a hand in judging MER model contests. He had a critical eye, dispensed helpful advice to both fledgling and experienced modelers, and counseled new modelers to test the waters of the model contest. He was a fine gentleman. Our aim with this special award is to encourage all NMRA modelers to participate in the national model contest.

So, who can participate? Any NMRA member in good standing, who has never entered a judged regional or national model contest, and who is not a professional model builder is eligible to enter this special contest.

The model must be entered in the judged model contest at the NMRA annual convention. The model contest must be judged using current NMRA Judging Guidelines. A modeler may also enter this special contest by having a proxy submit

the Entry Form, Judges Scoring Sheet, supporting documentation and the model.

The new modeler whose entry earns the highest score in the judged national model contest will be the winner. The award winner will receive an **engraved plaque and a cash prize of twenty-five dollars.** The Teese Award will be presented to the winner at the national contests award ceremony, Thursday morning of convention week. The national model contest manager, his deputy, or a representative of the award's sponsor, the MER, will hand out the Teese Award.

I am sure this award will interest a few of our members. So why not build a model for us? Keep notes of what you do – this will help you when you do the contest entry paperwork and if you are filling out the forms for the Achievement Program.

If possible, include a photograph or two, especially if you are building to the prototype. You might also want to show castings, fabricated parts, and other items as these can help explain how your model was built.

Remember, everyone can, if they choose to, enter the national model contest...you are really testing your skills against yourself, not the judges, other contest entrants, or whomever you might think will believe your work is not good enough. Try it once; I believe you will enter another, then another contest. 🏠



Achievement Program – Part 12

By Charlie Flichman, MER AP Manager

TO QUALIFY for all Achievement Program certificates, you have to fill out a Statement of Qualifications (SOQ). I mentioned the SOQ in every article I wrote about the various certificates. This article will shed some light on the SOQ.

The first step in filling out any SOQ is to be sure that you have the latest forms and the requirements for the specific certificate. The best place to get your forms is from the NMRA's Achievement Program forms web page—<http://www.nmra.org/achievement/apforms.html>. The top of page one of every certificate's SOQ form is basically the same. Your NMRA number and your expiration date are required. If you are a Life Member, please write LIFE on the "Exp" line. I think that the other lines are self-explanatory. The bottom part of page one and in some cases the top of page two, is a checklist of what is needed to apply for each certificate. The Gold Spike SOQ form (only one page) has a place at the bottom for the person verifying your application to sign. Your witness needs to be an NMRA member in good standing, so make sure he or she includes his or her NMRA number.

For the certificates that require a number of models (engines, rolling stock, structures, prototype models, etc.), you have to prepare the following:

- A. An attachment giving the detailed description of each of the models.
- B. Verification of the Merit Awards (copies of the Merit Award).
- C. The Statement of Qualifications form (2 pages).

One other item that is stated on most of the SOQs but not required are photos of the models.

In the case of models that have been judged, the Judge's Score Sheet will qualify as the detailed description for that model. If you provided other description paperwork (i.e., wrote "See Attached Sheet" on the Judge's Score Sheet), provide copies of this paperwork. On models that were not placed in a judged contest, you will have to write a description equivalent to what you would provide for the Judge's Score Sheet. Remember that the non-judged models have to be "super detailed," so it might be just as easy to enter them in a contest. They may not win a Merit Award but will probably come close, and you get formal recognition plus you have the paperwork. By the same token, you may have a model that you think is better than any that were submitted to a contest, but is sensitive to traveling to a convention or divisional meet. It can be one of your non-judged models, but you will still have to include the equivalent of a Judge's Score Sheet with your SOQ. Some models, such as structures, may have to be judged on your layout for various reasons. You need to prepare the paperwork the same as if you were bringing the model to a contest and a qualified person has to do the judging.

Make photocopies of the Merit Awards you received for your

models. Photographs of the models also helps the regional AP Manager verify your workmanship. In this day of digital cameras, this is much easier to do than in years past. On the second sheet of the SOQ you list the models that you have built to qualify for the certificate. The "Description of Model" is where you list the name of the model (i.e., C&O 2-8-0, D&RGW No. 3194 Boxcar, Coaling Tower, etc.) In the "Scratch-built" column, indicate yes or no for each model. Do the same thing in the "Merit Award" column. The next column is "Verified By." You don't need anything here for the models that have Merit Awards or have been in an MER Contest. Your Judge's Score Sheet qualifies as verification. For the models that are not judged, an NMRA member in good standing or your divisional AP Chairperson can verify that the model does exist and is your workmanship. This is another reason for providing a photo of each model, although the photo doesn't replace another member's signature. The date the other member is verifying the model goes in the "Date" column and their NMRA number goes in the "NMRA #" column. That's all there is to filling out an SOQ.

There is, however, a second method of providing sheet two of the SOQ form. It is the "Record and Validation" sheet. It is just another way for you to keep track of your qualifying models. As a personal choice, I prefer page two of the SOQ as it has a place to put the regional certificate number on it. Every certificate earned has a regional number assigned to it.

Completing the Statement of Qualifications for the rest of the certificates basically follows the procedure described above with minor variations. For the Scenery, Civil, and Electrical certificates, a person who has already earned the respective certificate or an MMR may judge your work. Judging for the Scenery certificate normally would be done at your home or club, unless you bring modules to a meet. Here again a copy of the Merit Award, photos (or video), and a description are required. On page two of the Civil SOQ, there is a place for the judge to sign for your three pieces of scratchbuilt trackage. Likewise, on the Electrical SOQ, the judge has to sign for the satisfactory demonstration and operation.

The Chief Dispatcher SOQ is five pages long. Page three is a Record of Operation, while page four is the Record of Activities. In this Record of Activities, the "Verified By" person may be either a layout owner or a club member as long as they are NMRA members.

The Association Official and Volunteer SOQs provide spaces to fill in the different positions that you have held. The "Verified By" person here is usually the next level up from the job you held, i.e. a Committee Chair, Convention Chair, Regional President, etc. The Author SOQ is similar to the ones for Official or Volunteer. You list all of your publications, clinics, and videos, and since you have to provide copies of the same, there isn't any need for verification. The regional AP Manager will check that your points add up to the same score that you obtained.

Last but not least is the MMR SOQ. I think that this form is

pretty straight forward, and if you have a problem with this one after doing the other seven or more—Well!

I hope this hasn't been too confusing. When I first mentioned doing this article to several people, they assured me that it was needed. If you have any questions that I haven't covered here, please feel free to contact me at cwflich@aol.com about them. This reaches the end of the AP articles for now. I hope they have been of value to those of you who are now participating in or want to participate in the Achievement Program. If you have not participated yet, try it, you might surprise yourself and like it. 📧

Rails to Raleigh Special Contest Awards

*By Ray Bilodeau,
MER General Contest Chairman*

THE MID-EASTERN REGION of the NMRA will hold its annual convention, October 27 to 30, 2005, in the Raleigh, North Carolina area. This is a reminder to all our NMRA members who are thinking of attending "Rails to Raleigh," that we have a full slate of contests planned for the weekend including a judged model contest, and popular vote photo and arts and crafts contests. There will be several special awards given in the judged model contest. These will include: the **President's Award**—the topic—maintenance-of-way. President Hyde will choose the winner. The host committee has decided to offer the **Pride of Dixie Award**...an award with a particularly "Southern" flavor. The model, a barn, will be a judged category in the model contest. We will also offer the **Clyde Gerald, Blue Lantern, Philadelphia New Modeler, MER Narrow Gauge, Marv Kershner, Ma & Pa Modeling Award** and **Favorite Train Award**. The photo contest will continue its tradition of presenting an award to the judged **Best of Show Photo**.

More to come next month about these and other contest tidbits; you might also look for contest information at the convention website: www.trainweb.org/cpd13/mer05. If you have any questions or comments contact me at: (302) 636-0888, or RaymondBilodeau@aol.com. 📧

Mid-Eastern Region, Director Statement of Candidacy



**Richard (Dick) Genthner,
MMR**

I would appreciate the opportunity to serve as a Director for the Mid-Eastern Region for a number of reasons.

I have been involved in model railroading for over 60 years and have been a member of the NMRA for almost all that time. Since retiring and moving to North Carolina in 1993, I have been a member of the MER and am a charter member of the local Carolina Piedmont Division 13. In addition to serving as CPD 13 Superintendent for several years and as a judge in MER model contests, I serve as the local Achievement Program chairman and completed the requirements for Master Model Railroader in November 2003.

I believe that the local Divisions are at the frontline in building interest among all model railroaders including those who are not members of the NMRA. I believe that an active Achievement Program can be part of the solution to the membership problem. The future of the NMRA/MER rests in active local programs and the MER should promote the Divisions as the frontline of the "World's Greatest Hobby."

I would appreciate your vote to serve you as a Director of the Mid-Eastern Region.



Bob Glista

My name is Bob Glista. I grew up in West Caldwell, New Jersey and now reside in Bethesda, Maryland. For the last twenty-five years I have been working my own Insurance Agency in Rockville, Maryland. I attended American University in Washington, DC and hold a degree in Political Science.

I have been involved in several clubs and organizations in the past years, the highlight being Optimist International. In the local Optimist club I have held the positions of Board Member, Chairman of the Board, Project Review Committee, Project Review Chairman, and President. This local club has a seventy-five year history and spans Washington, DC, northern Virginia, and the Maryland suburbs.

My interests are travel, model railroading, and flying. I have held a private pilots license since age seventeen.

For the last seven years I have been a member of NMRA, MER, and Tidewater Division. I have attended all but one NMRA convention, and all but two MER conventions and regularly attend the Tidewater Division meets. I am currently modeling HO scale but I am interested in all scales. When it comes to railroading, I enjoy modeling just as much as prototype and railroad history.

As Director, I would take a great interest in doing my best to make the MER run smoother and more efficiently as well as staying focused in keeping our organization on a common goal in representing all scales.

continued on page 12

Mid-Eastern Region, Director Statement of Candidacy



Herbert E. Gishlick

Herb Gishlick is a professor of economics at Rider University in Lawrenceville, NJ. He has been a model railroader for over 50 years and has a cotemporary-era HO layout in his basement.

He has been an NMRA member since 1956, an NMER member since 1973, and is active in the New Jersey Division. He has given clinics at national and regional NMRA conventions and New Jersey Division meets. He has earned achievement certificates for motive power, civil engineer, association volunteer, author, and scenery. His layout is scheduled to be on one of the 2006 layout tours for the Philadelphia NMRA convention.

Over the years, Herb has served as an officer for numerous professional and service organizations and welcomes the opportunity to serve the MER if elected.



Chuck Hladik

As Director I would like to see the Divisions expand their membership and also get more people involved in the Achievement Program. There already are many great modelers in the Divisions that aren't getting their just recognition.

As a Board member I still consider myself a Division member first, but we must govern for the betterment of all. We visit Divisions and get no feedback at the time, but later, WOW. Step up; it only takes one voice to try to accomplish something.

As one of your Directors I want to hear from you and will try to present your views to the Board. Don't pass up your opportunity to be heard. VOTE!!

Sincerely,
Chuck Hladik
James River Division



Mark Sisk

My name is Marc Sisk and I am seeking the position of Director on the Mid-Eastern Region's Board of Directors. I have been a model railroader for many years now and a member of the NMRA since 1994. I am currently the Clerk of the Potomac Division of the MER, the Secretary of the Northern Virginia NTRAK (the largest NTRAK club in the country), and a board member of the Fairfax Station Railroad Museum. I currently live in northern Virginia. I have attended several NMRA conventions at all levels, presenting clinics and judging in the model contests. I am actively involved in the pursuit of Master Model Railroader with two certificates done and a third nearing completion. I fully support the goals and direction of the NMRA and of the MER. With many years as a manager and leader in the military and now in civilian life for the past five years, I feel that I can be a definite asset to the Board and would enjoy serving as a Director. Your vote would be appreciated.

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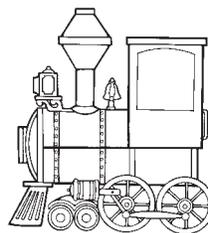
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Mid-Eastern Region
2005 Annual Convention**

October 27 – 30, 2005
Embassy Suites Hotel,
Cary, North Carolina



Hosted by the Carolina Piedmont Division

Make plans to join us in North Carolina for a fantastic model railroading weekend!

Visit area model railroad layouts

- More than 25 layouts will be open for self guided tours over three days – from Friday morning through Sunday afternoon
- Most scales represented – N, HO, O, G, live steam and garden railway

Increase your knowledge and skills

- Three days of clinics on various topics and “how to” presented by experienced model railroaders
- Extra fare clinics planned – DCC, scratch building, weathering

Enter a contest

- NMRA sanctioned model and photo contests with awards presented at the Saturday evening banquet

Tour the North Carolina Transportation Museum

- Ride on “The Piedmont”, a passenger train operated by the North Carolina DOT, to Salisbury, NC
- Visit the North Carolina Transportation Museum at the Southern Railway’s Historic Spencer Shops

Go on a prototype tour (tentative opportunities – subject to availability)

- New Hope Valley Railway – Eastern NC Chapter of the National Railroad Historical Society
- North Carolina DOT rail yard or the Norfolk Southern Glenwood Yard

Attend a special event

- Saturday evening night banquet and awards presentation
- Operating sessions on layouts
- Model train show on Saturday and Sunday

Go here for convention information

- Website: www.trainweb.org/cpd13/mer05

Hotel information

Embassy Suites Hotel
Raleigh-Durham-Research Triangle East
201 Harrison Oaks Boulevard
Cary, North Carolina 27513
Phone: 919-677-1840
Toll-Free: 1-800-EMBASSY
www.embassysuitesraleighdurham.com

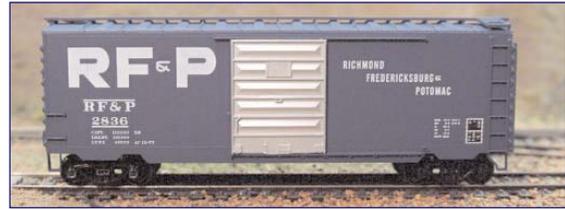
Convention rate: \$ 89.00

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COURTESY OF THE FREIGHT YARD

These 40' Western Maryland N-Scale box cars are Micro-Train bodies custom painted by The Freight Yard. They are box car red with prominent white "speed lettering".



P.L. MATTHEWSON (2)



These 40' RF&P PS-1 HO-Scale box cars are Accurail bodies, custom painted by Accurail. They are blue with aluminum-colored doors, and feature prominent white lettering.



This HO-scale building kit is designed around E.L. Moore's "Bunn's Seed & Feed" building which appeared in the 8/73 issue of Model Railroader. The kit features laser cut wood components as well as other parts and castings.

WESTERN MD. N-SCALE CARS	QUANTITY	MEMBER PRICE*	NON-MEMBER PRICE	TOTAL
#29290	_____	\$19.95	\$23.95	\$_____
#29299	_____	\$19.95	\$23.95	\$_____
Shipping \$2.50 for first car, + \$1.50 for each additional car				\$_____

RF&P HO-SCALE CARS	QUANTITY	MEMBER PRICE*	NON-MEMBER PRICE	TOTAL
#2836	_____	\$11.95	\$14.95	\$_____
#2874	_____	\$11.95	\$14.95	\$_____
Shipping: \$2.00 for each car (not for each order)				\$_____

* Please list your MER Membership # _____

MER CLOTH PATCH**	QUANTITY _____	\$3.00 including S&H	\$_____
MER LAPEL PIN/TIE TACK**	QUANTITY _____	\$6.00 including S&H	\$_____
MER HO-SCALE STRUCTURE KIT** ("Bunn's Seed & Feed")	QUANTITY _____	\$75.00 postpaid	\$_____
THE Locals** — (all issues 1997-2004) on CD	QUANTITY _____	\$5.00 postpaid	\$_____
DONATION			\$_____
TOTAL AMOUNT ENCLOSED			\$_____

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Achievement Program Update

By *Charlie Flichman*
MER AP Manager

Since the last report in **The Local**, the following Achievement Program certificates were earned and awarded:

Division 2 - Potomac

Clint Hyde – Author

Division 10 - South Mountain

Jane J. Clarke – Gold Spike
Bob Van Zant – Gold Spike
Robert L. Proctor – Gold Spike

Division 11 – Susquehanna

Alan Mende – Association Volunteer

Division 12 – Carolina Southern

Rick Knight – Engineer – Electrical

MER At Large

Donald R. Woodwell – Author

In a perfect world, this information will appear soon in **Scale Rails**. This should not deter you from giving recognition locally. Normally you will be able to recognize AP accomplishments long before the names appear in **Scale Rails**. 

Calling All MER Members!

Don't forget about Independence Junction (<http://www.ij2006.org/>), the NMRA's 2006 National Convention. The Mid-Eastern Region will host it July 2 to July 9, 2006 in Philadelphia, PA. There is a great need for volunteers to help at the convention. If you would like to help out, go to <http://www.ij2006.org/volunteer.html> and fill out the volunteer survey form. Be a part of this amazing event! It could get better with your help! Not only will you get great satisfaction from participating, you'll also qualify for AP Volunteer points. 

Positions Available

MER has three positions open:

- **Editor of The Local**
- **Official Photographer**
(primarily you shoot model contest photos)
- **Webmaster**

Please contact Clint Hyde to apply, chyde@cox.net. 

SHARE YOUR EXPERTISE

(and earn some AP Points at the same time!)

PRESENT A CLINIC

PLAN AHEAD AND SHARE your knowledge in an area you enjoy. We need your expertise in supporting our upcoming Mid-Eastern Regional Convention to be held October 27–30, 2005 in Cary, North Carolina. Clinic times are wide open and preferences will be given on a first come, first serve basis. Presentations will be a minimum of one hour and can be longer depending on the subject and presentation format. A handout is required providing a clinic outline and important points. Certificates of Appreciation will be presented to clinicians at the conclusion of their clinics.

Interested in the NMRA Author Achievement Certificate? You can receive four (4) points for your Clinic. A handout must be a part of the presentation and must be included in the certificate application. If you can present your clinic a second time or more, you can receive two (2) points each time towards the Association Volunteer Certificate.

Please feel free to call or e-mail me about presenting your clinic at the “Rails to Raleigh” Convention. Be a participant and help make our convention a rousing success. My e-mail address is g249sing@hotmail.com, or call me at (919)-931-0946.

Thanks.
Gene Sing 

